

BOAT

International

>> WORLD EXCLUSIVE <<

SOLO

STEP ON BOARD TANKOA'S NEW 236FT STAR

+ DESIGN REPORT:
THE HOTTEST
INTERIOR TRENDS



REFIT FOCUS
The incredible story
of *Haida 1929*



POWER PLAYERS
The names making
waves this year



ShowBoats
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DISPLAY UNTIL 01/10/2019

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Haida 1929 Page 116

Beam \updownarrow 29' 11"

Gross tonnage
720



Cruise speed
11 knots



Engines
2 x 750hp Krupp EV 40



Designers: Cox & Stevens; Adam Ley
Construction: Steel hull and steel/timber superstructure



Built: Germany | Launched: 1929; Refit: 2018
Shipyard: Krupp Germaniawerft
Refit shipyard: Pendennis

13 guests 

18 crew 

Length: 233' 3"

Cynthia Page 144

Beam \updownarrow 34' 1"

Gross tonnage
782



Max speed
15 knots



Engines
2 x Cat 3512B

10 guests 

14 crew 



Designers: De Voogt Naval Architects; RWD; Todhunter Earle Interiors; Claudette Bonville Associates
Construction: Steel hull and aluminum superstructure



Built: The Netherlands | Launched: 2005; Refit: 2018
Shipyard: Feadship; Refit shipyard: Derecktor

Length: 182' 1"

Solo Page 92

Beam \updownarrow 38' 1"

Gross tonnage
1,600



Max speed
17.5 knots



Engines
2 x Cat 3516B

12 guests 

18 crew 



Designers: Vincenzo Ruggiero; Tankoa Engineering; Francesco Paszkowski Design; Margherita Casprini
Construction: Steel hull and aluminum superstructure



Built: Italy | Launched: 2018
Shipyard: Tankoa Yachts

Length: 236' 3"

Pardo 50 Page 33

Beam \updownarrow 16'

Displacement
15 tonnes



Max speed
38 knots



Engines
2 x Volvo IPS 600

20 passengers 



Designers: Zuccheri Yacht Design; Cantiere del Pardo
Construction: Composite hull and superstructure

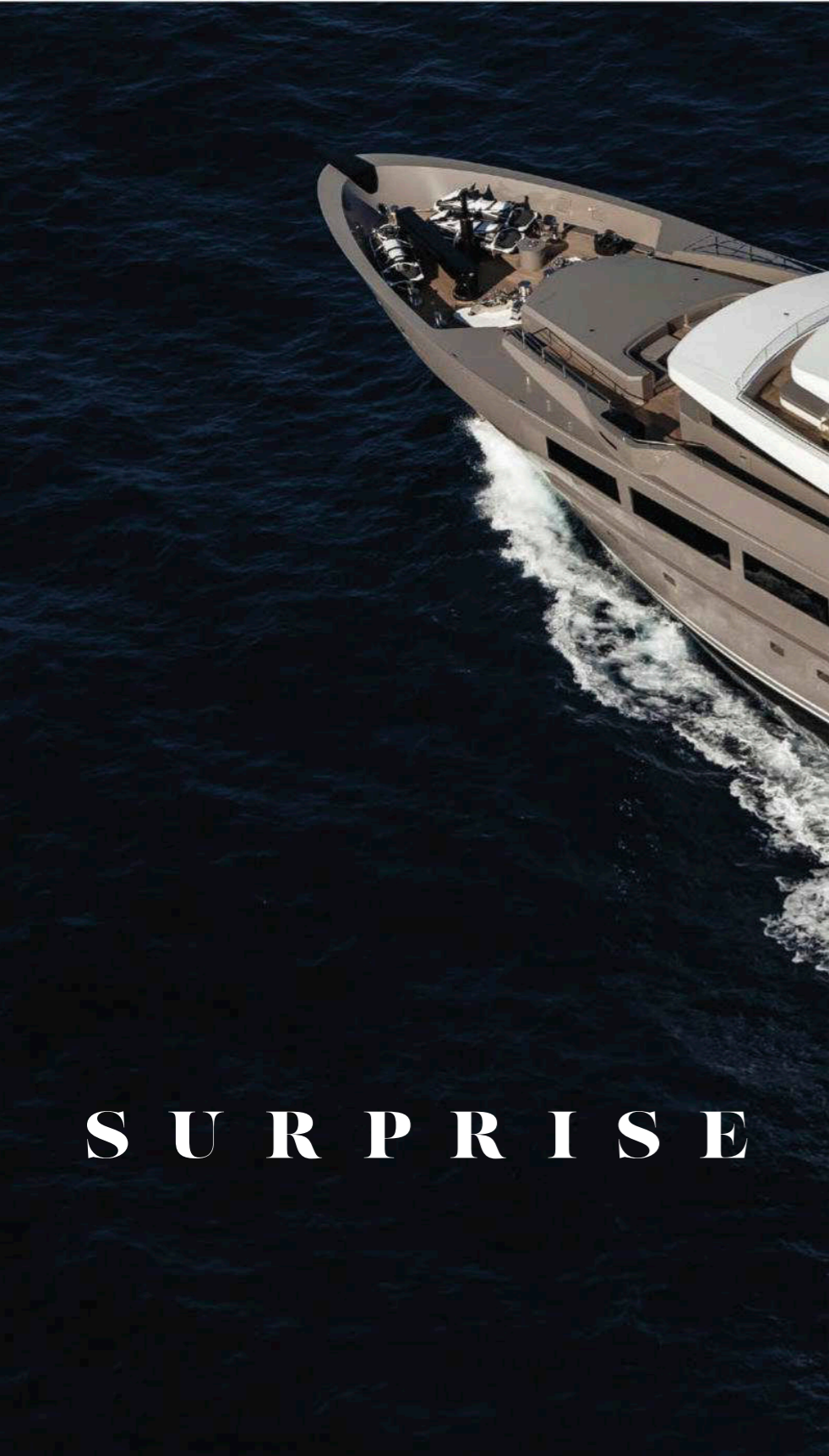


Built: Italy | Launched: 2018
Shipyard: Cantiere del Pardo

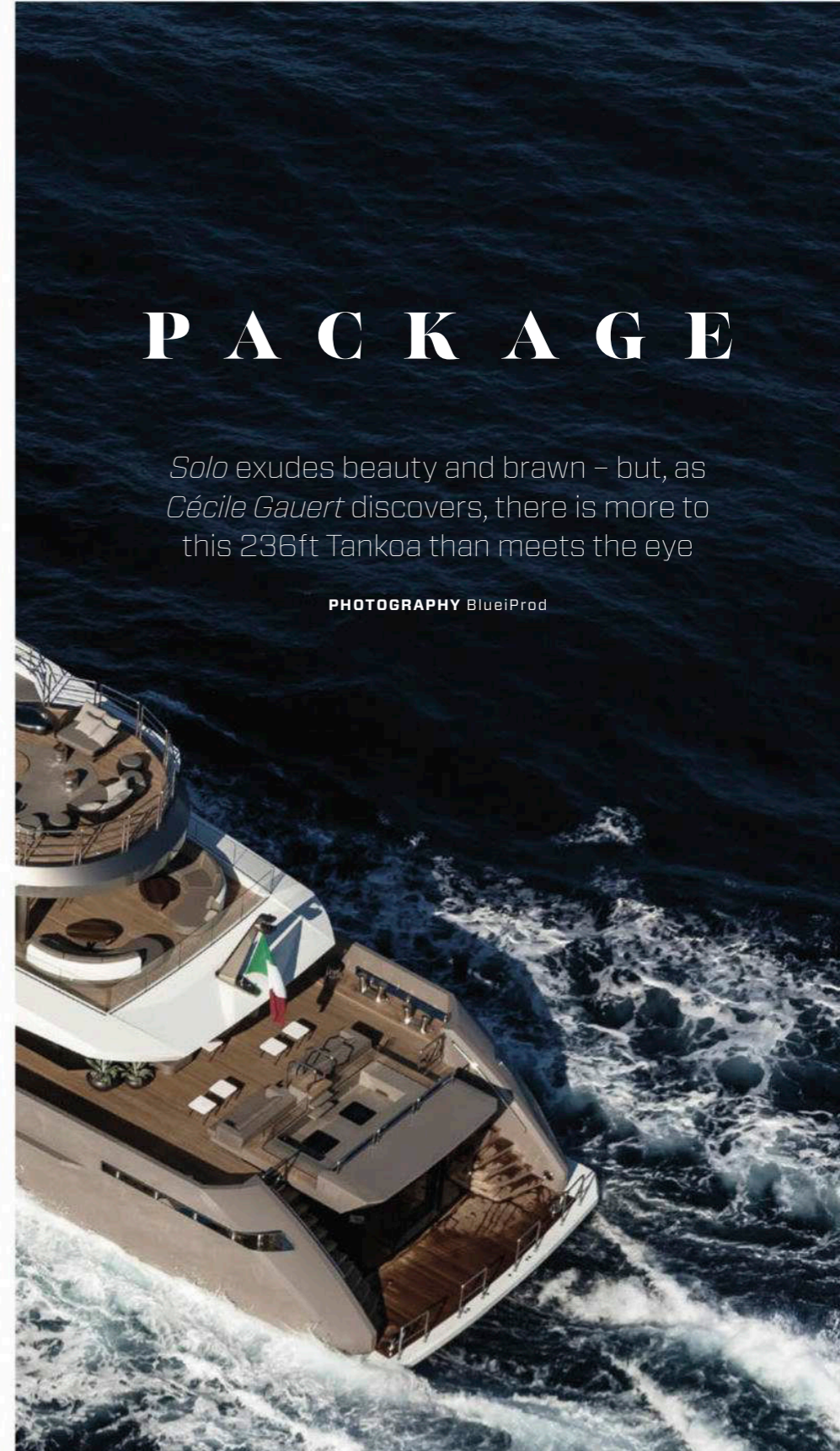
Length: 53'



30ft



S U R P R I S E



P A C K A G E

Solo exudes beauty and brawn – but, as *Cécile Gauert* discovers, there is more to this 236ft Tankoa than meets the eye

PHOTOGRAPHY BlueiProd

technologies and market trends," he says.

He asked for "solid comparisons between the yard's initial straight diesel offering and a diesel-electric/hybrid solution," McIlroy adds. He did not have a preference, he just wanted the facts, and those told him that a traditional propulsion set-up would be more efficient. For a vessel of this size, "for the same speed, if you have a diesel electric system, the consumption is about 15 percent more," Ruggiero says.

Results in hand, the owner and shipyard decided to carry on with the

once again part of the team and worked closely with the shipyard, owner and naval architect on the development of *Solo*, Tankoa's third delivered yacht.

"A couple of issues were fundamental for the whole design and layout," Paszkowski says. "Although *Solo* is similar to her 69 meter sistership, her three meters of extra length and underwater exhaust allowed a gain of volume on every deck. Both solutions enabled us to design a layout with large social spaces inside and outside."

The hull extension, at the stern, not only helped interior volume but also had an important fringe benefit in terms of efficiency. The hull, which has an unusual shape, with a wider section amidships, also has a high length-to-beam ratio. The team left nothing to chance, and following CFD analysis the naval architects also did extensive tank testing that helped fine-tune the placement of the exhaust vents and all appendages. The results were satisfying in terms of efficiency, but also for seakeeping.

"For stability, the boat is amazing," says build captain Renzo Chelazzi, who took

A T F I R S T

SIGHT, nothing about Tankoa's new 236ft superyacht *Solo* screams "green." This is a large steel yacht powered by twin Caterpillar engines and triple generators, with no hint of an electric motor, solar set-up or exotic propulsion system – just a pair of traditional shafts driven by diesel, ending in two five-blade props. On board, meanwhile, is every conceivable luxury – six cabins, a beach club, saltwater pool, two hot tubs, 18 crew and a tropical fish aquarium. Yet Italian classification society RINA has bestowed *Solo* with its Green Plus Platinum notation, the highest level of environmental certification.

Then you notice the yacht's range, 9,100 nautical miles at 10 knots – that's expedition yacht territory. Vincenzo Ruggiero, *Solo*'s naval architect, explains that the yacht is even more fuel efficient with her standard diesel arrangement than if she were powered by a diesel electric system. This is the owner's third yacht, and second Tankoa, and he is very knowledgeable, says Albert McIlroy, the owner's project manager. "Although he has very limited time to use his yacht due to his business occupations, his passion for yachts drives him to constantly monitor the next

project using a hull that had been started on spec in 2010. It had been designed to be the same length as Tankoa's previous launch, 227ft *Suerte*, but the owner wanted changes. He asked principally for more accommodation space, with the goal to make his next yacht a great option on the charter market. Francesco Paszkowski, the designer of *Suerte*, was

LET THERE BE LIGHT

A custom made chandelier in bronzed mirror tiles lights up the dining table while laminated glass panels in the bulwarks enable sea views for guests seated at the table



THE DECOR IS BOTH MODERN AND MASCULINE: BLACK OAK FLOORS, DARK GRAY OAK WALLS AND MACASSAR EBONY ACCENTS ON CUSTOM FURNITURE



BLACK AND WHITE
Large windows and light hues balance darker finishes in the sky lounge (right), the VIP stateroom (below), beach club (center) and “winter garden” and guest reception area (bottom)



Solo to the Monaco Yacht Show. “On the way back, we had strong northerly winds, we experienced 40 knots, but we didn’t feel it,” he says, switching on a video on his smartphone that shows her effortlessly gliding through dark water fringed with white foam. “We were going 14 knots.”

Fuel efficiency – and seakeeping – were not the only objectives, however. Strict new rules around emissions and water pollution have arrived in the shape of the International Maritime Organization’s Tier III requirements. These limit emissions and water pollution from yachts and require that most diesel-powered vessels built from 2016 (the keel serving as benchmark) carry a nitrogen oxide (NOx) mitigating system in order to navigate in Emission Control Areas (ECAs). Tankoa is an ambitious yard that from the get-go stated its goal to compete with the world’s best. Although it did not have to just yet, the yard decided to go ahead and make *Solo* one of the first Tier III compliant yachts afloat.

Working closely with Ruggiero on the engineering side, it assembled the best technology currently

available to lower as much as possible its impact on the environment. A big part of this – and an obvious presence in the engine room – is a selective catalytic reduction (SCR) system. SCR is a chemical process that significantly reduces NOx by injecting a solution of urea into the engine exhaust. Tests showed that the system by Ecospray Technologies that Tankoa installed reduces NOx emissions by an average of 80 percent, Chelazzi says – meeting Tier III requirements.

To deal with the exhaust from the two 230kW Northern Lights generators,



BALANCING THE DARK FLOOR AND WALLS ARE LIGHTER HUED FABRICS AND COLORFUL PIECES OF ART

Tankoa installed a Hug Engineering particulate filter and a Nauticlean converter for the third, smaller generator. A monitor allows the engineer to keep an eye on carbon dioxide levels at all times. The yacht also carries systems to guard against potential water pollution. An Erma First ballast filter system removes all pollutants before any water is released. “You cannot discharge ballast within 200 miles of the coast of another country when you’re coming from abroad. In the water, there are a lot of micromaterials you don’t want to carry from one to another area,” Chelazzi explains. The yacht also is equipped with ultrasonic antifouling in vulnerable areas, such as the sea chest, to keep barnacles at bay without the use of biocides.

“What was done here was much more

than was required at the time of the keel laying,” Ruggiero says. “That is the philosophy for all the boats that we do, to build them more like small passenger ships.”

Solo may be built like a passenger ship, but she does not look like one. “Modern and timeless,” as her owner wanted, she is a notable new addition to the charter fleet. The view from the very top deck, a private aerie with spacious sunpads under the mast, includes a beautiful succession of decks unfolding below, complete with a helipad that doubles as a dance floor. From the shapely spa pool, one of two on the owner’s deck, the attractive

arrangement of deck furniture by Summit and Tribù, to the seawater pool wrapped in curved glass on the main deck, the design is harmonious.

“My favorite space on board is the upper aft deck al fresco dining table and lounge areas; it offers privacy to the guests in a cozy and comfortable environment open to breeze and with nice views,” says the yacht’s charter manager, Nicolas Fry of Northrop & Johnson.

“I am also a big fan of the oversized beach club. At nearly water level, it offers outstanding views and is complete with a lounge, a bar as well as a sauna with private balcony, a hammam and a gym with another foldout balcony.”

Paszkowski says the beach club is a key feature. Entered from the swim platform through a large watertight glass door, it is





Designer's pick

"The place I like most is the deck dedicated to the owner's master suite," Paszkowski says. The stateroom, located in front of the huge floor-to-ceiling windows, looks like an island. It is. The island is separated from a huge apartment that includes two wardrobes, an office and a bathroom. From the bed the owner can enjoy unrivaled panoramic views over a private spa pool and on to the sea. "I like how natural light balances the dark colors of the decor, which were non-negotiable. I think that the light fabrics that we used and the amount of light coming from huge windows and skylights create a perfectly balanced combination," he adds. "Outside, the owner can have breakfast in a nicely secluded area near the pool. From the helipad aft, he has direct access to his apartment. I think we met the owner's wish for privacy.

"There is another space on this deck that the owner can use as a relaxation area, just outside of his apartment," Paszkowski continues. "As it is close to a bathroom, it can be converted into another guest cabin, which meets the owner's requirements for flexible spaces and for charter use."



DOUBLE DUTY

The helipad aft on the owner's deck (top and opposite page) also makes a party deck. Above: the owner's stateroom boasts a ceiling height

of more than eight feet. The bed looks forward over one of the deck's two hot tubs. The second one is adjacent to the helipad





lit up from a skylight at the bottom of the pool on the main deck. The hammam features Tuscan calcatta marble and a shower cooling guests in a hurry with water maintained to a chilly 41 degrees. The beautiful wood in the sauna had a previous life inside a medieval castle in Finland.

That is probably the only part of the lower deck that guests will ever see. The rest of it is dedicated to the tender garage, two-tier engine room and then the spacious crew area. There is nothing lacking in the yacht's underbelly, including a galley designed by Italian restaurant professional Marrone and run by a chef with experience in Michelin-starred restaurants, a big laundry area, cold rooms, freezers and fridges and a garage well stored with toys, to support an active charter program.

"Solo's owner had precise ideas about how his yacht should be used," Paszkowski says. "Large social areas outside and inside were very important to him. Privacy was also essential." To fulfill this request, Paszkowski designed an owner's deck that can be cut off from the rest of the guest areas with a private access code unlocking the elevator or door to private stairs.

The owner, who chose the custom color that Awlgrip created for the hull,

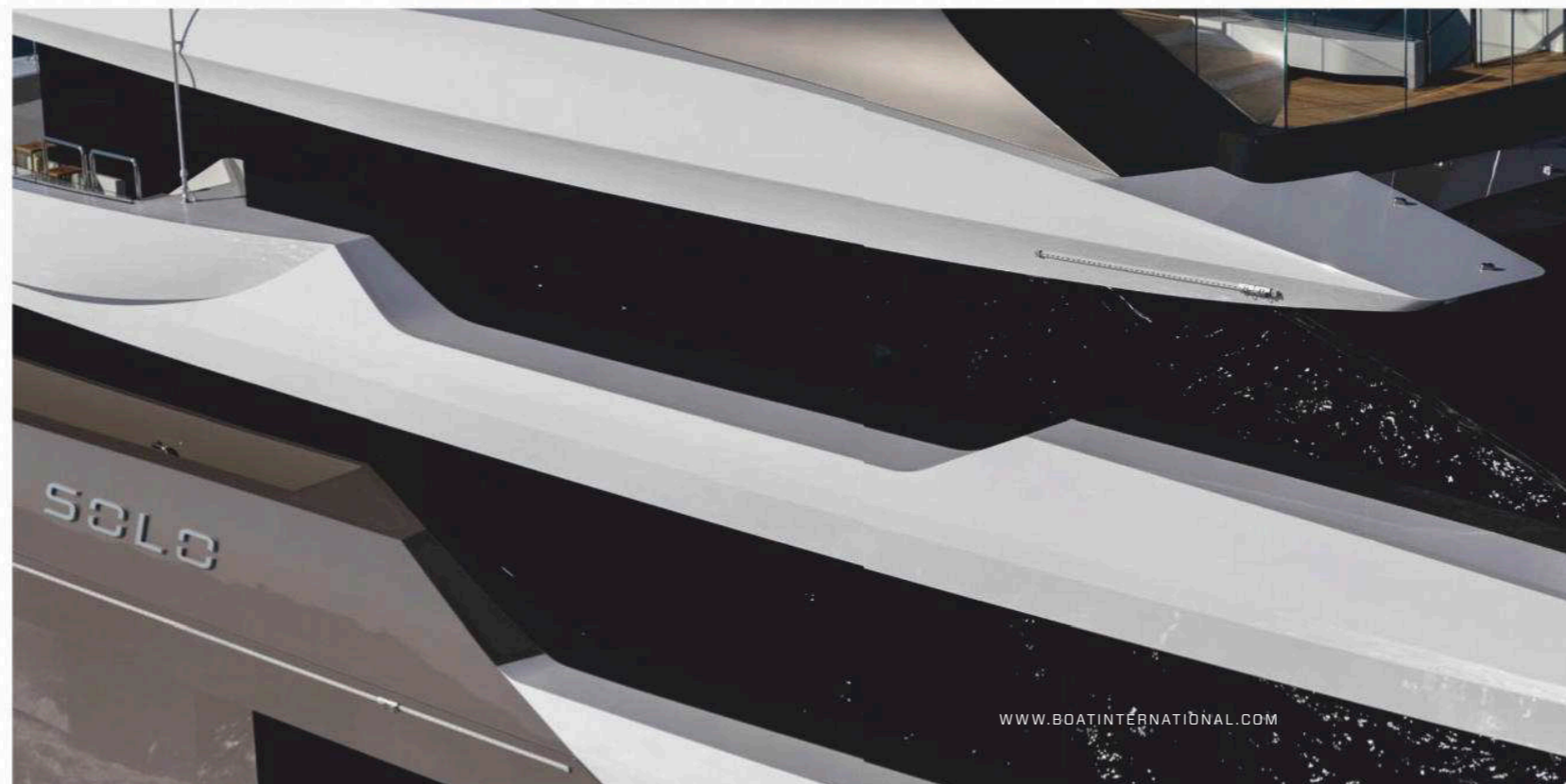
expressed his preference for darker hues. Created in close consultation with the owner and Margherita Casprini, the decor is both modern and masculine. Floors are in black oak, dark gray oak is on the walls, and Macassar ebony accents custom made furniture, along with cappuccino marble from Rajasthan. A striking forest black antique marble from France covers the beach club area floor, a stairway and the owner's bathroom.

Balancing the dark floor and walls are lighter hued fabrics, colorful art, backlit white onyx on the bars, plus amazing windows, low bulwarks and expansive outside decks drenched in sunlight. Views and transparencies were an important part of the design. Bulwarks are low and cut in places to maximize views, and frameless glass panels replace stainless-steel stanchions all around the sundeck. They tame the wind and provide safety but do not interrupt the gaze.

After a few hours basking in the sun, heat and salted sea air, the nicely chilled darker lounges of *Solo* must feel like a delicious reprieve.

A favorite in the evening will likely be the spacious sky lounge/piano bar offering plenty of seating on low and deep sofas by Fendi Casa in front of a 90-inch 4K OLED screen above the fireplace. One of the eye-catching features in the main deck dining room, meanwhile, are colorful fish in an aquarium by Melik designed to keep the water still even as the yacht moves. The crew say they do not have to worry about maintenance much as a self-filtration system keeps the tank clean, but they have easy access to all the equipment stored in a small dedicated room off the main lobby.

One of the similarities with *Suerte* is found on the aft section of the main deck, described as a winter garden. Access to the main deck salon is through two side doors, instead of the more usual big central sliding door. Here is a welcome lobby and waiting area with low seating, flowing curtains, a bar and a large planter filled with cacti. It seems like a perfect place to wait before boarding the yacht's two Dariel tenders. But then again, with all that *Solo* has to offer, why would anyone want to leave? ■



“THE UPPER AFT DECK IS MY FAVORITE SPACE ON BOARD – IT OFFERS PRIVACY TO THE GUESTS IN A COZY AND COMFORTABLE ENVIRONMENT”

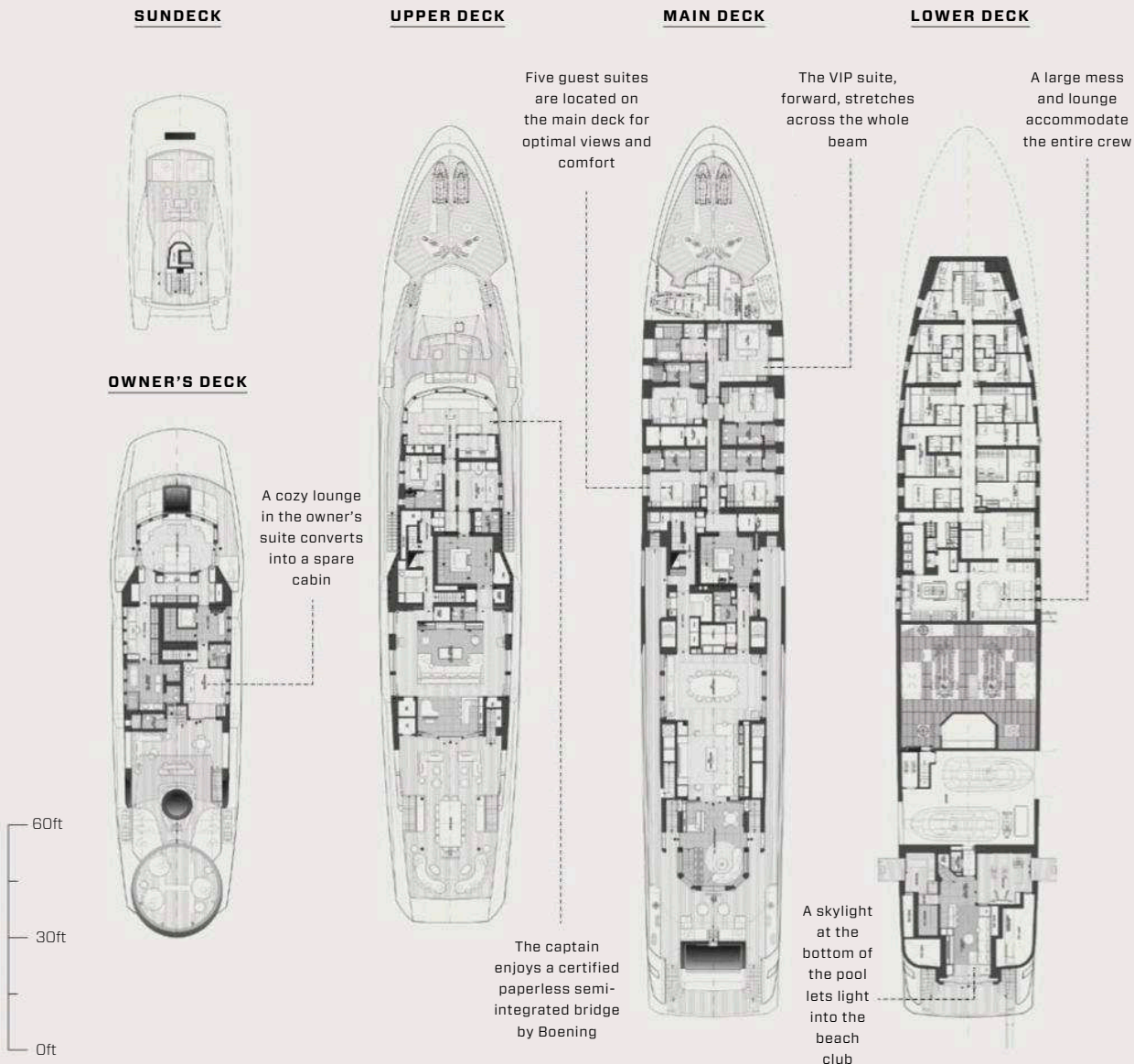
TEST OF TIME

The owner's brief was for a modern, timeless yacht, both private and perfectly suited for charter. Paszkowski achieved the elegant exterior design through what he calls "an accurate combination of curved and clear lines and the selection of a distinctive hull color"



SOLO

TANKOA



LOA 236' 3"
LWL 191' 2"
Beam 38' 1"
Draft 11' 1"
Gross tonnage
 1,600GT approx
Engines
 2 x 1,825kW CAT
 3516B DITA SCAC

Speed max/cruise
 17.5/15 knots
Range at 10 knots
 9,100nm
Generators
 2 x 230kW Northern
 Lights
Fuel capacity
 42,800 gallons

Freshwater capacity
 9,933 gallons
Tenders 2 x 25'
 custom Dairiel
Owners/guests 12
Crew 18
Construction Steel
 hull and aluminum
 superstructure

Classification Lloyd's
 # 100 A1, SSC, Yacht,
 Mono, G6, # LMC, UMS,
 and RINA: C # HULL,
 • MACH, Unrestricted
 Navigation, YCH,
 GREEN STAR, MCA LY3
Naval architecture
 Vincenzo Ruggiero

Exterior styling
 Francesco Paszkowski
 Design
Interior design
 Francesco Paszkowski
 Design; Margherita
 Casprini

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